

Melrose Pedestrian & Bicyclist

Committee

**Lower Main Street
Safety & Access Enhancement
Recommendations**

May 2020

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Introduction

The Melrose Pedestrian & Bicyclist Advisory Committee is an volunteer-led group with a mission to improve street safety and access for all roadway users in Melrose. The Committee published a [North-South Bike Route](#) recommendations plan in 2018, which the City of Melrose used as the basis for implementation of a MassDOT Complete Streets grant. This earlier report provided recommendations for improvements for people walking and bicycling along two routes—the "commuter route" along Main Street and a "low-stress" route to the west of Main Street—from the Wakefield border to West Wyoming Avenue.

The Committee subsequently prepared this report with recommendations for a continuation of these two routes, from West Wyoming Avenue to the Malden border. See Figure 1 for an overview of the route alignments.

The recommendations in this report are based on careful observation and transportation industry best practices. These recommendations include low-cost striping proposals that can be implemented with planned 2020/2021 repaving and restriping of the roadway. There are also several optional configurations that will provide more robust safety and access improvements, should funding become available. These include the addition of separated signal phases, that will require new signal hardware. A phased approach to implementation will allow the majority of benefits to be implemented in the near term, with subsequent elements added as funding and/or project coordination opportunities are identified.

The Committee remains eager to work with the City of Melrose Administration, Department of Public Works, and City Council, to refine the recommendations in this report, and implement improvements that will help to improve safety for all road users of all ages and abilities.

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W. Wyoming Avenue

Design Recommendations

The Wyoming Hill Neighborhood is the densest neighborhood in Melrose. It hosts a commuter rail station, restaurants, commercial businesses, Lincoln Elementary School, and an increasing number of multi-family housing units. The City has installed crosswalk improvements, including RRFBs, and is still planning to upgrade crosswalks with median refuges with flexible “safe-hit” bollards, based in part on the recommendations made by our earlier North-South Bike Routes report. The Committee endorses installation of these features at both crosswalks (at Berwick Street and at Waverly Place); in addition, the Committee recommends daylighting in advance of the crosswalks and at intersections, to make sure people driving can see people entering the crosswalks and other vehicles. See Figure 2 for design details.

On eastbound W. Wyoming Avenue at Main Street, adding a dedicated left turn lane with a protected left turn signal will help to ease congestion on W. Wyoming, which could also help reduce frustration and dangerous driving behavior.

There is currently a 7-second delay for the westbound traffic on East Wyoming, however this is not apparent to eastbound motorists. A protected left turn signal would make better use of this time by ensuring eastbound motorists know they can turn left before the oncoming traffic proceeds. The signal will require a yellow and red phase, but can then return to flashing yellow for the duration of the straight/right turn green. A similar configuration exists at Lebanon Avenue and Main Street. A traffic study is necessary to ensure the storage on Main Street is adequate to accommodate additional vehicles from W. Wyoming Avenue.

To fit this recommended left turn lane, two parking spaces must be removed on the north side of W. Wyoming Avenue. With two 11-foot wide travel lanes and a 10-foot wide left turn lane, there is 5-feet of space remaining, which can be used as a bike lane with a bike box at the intersection for cyclists who want to make a left turn. There is already a no turn on red at this intersection, which should remain.

West Wyoming Ave at Waverly Street, looking west



Lower Main Street

Parking Analysis

The parking on lower Main Street is significantly underutilized, with the exception of a few key blocks. To understand exactly where parking is used, and where it is not, the Committee recorded the number of cars parked between W. Wyoming Avenue and Island Hill Avenue (just beyond the border of Malden) over the course of a week in 2019, from September 6th to September 14th. This week was selected because it was after the Labor Day holiday, and included two Saturdays and all the weekdays in between. Counts were taken at a variety of times, including morning, afternoon, and evening.

The results of these counts are shown in Appendix A, accompanied by photographs of several blocks taken at the time of the counts. Table 1 provides a summary of the week's parking counts. The key below describes each block location, which are also shown on Figure 1.

On average, the two highest-use areas are both on the west side of Main Street:

W. Wyoming Avenue to Potomac Street (900 lineal feet of curb)

- Average: 6.5 cars
- Maximum observed: 13 cars
- Minimum observed: 2 cars

Banks Place to Island Hill Avenue (1400 lineal feet of curb)

- Average: 5.3 cars
- Maximum observed: 18 cars
- Minimum observed: 0 cars

Together, these two high-demand on-street parking segments average about 12 cars over 2300 feet of lineal curb. The entirety of the remaining corridor measures 6600 feet but averages only 6 cars total. **In other words, an average of just 6 cars are parked along over 1.25 miles of curb of lower Main Street.**

Table 1: Parking Analysis Summary

Block #	Individual Blocks				Consolidated Blocks			
	SB (West Side)		NB (East Side)		SB (West Side)		NB (East Side)	
	Ave Occup.	# times recorded	Ave Occup.	# times recorded	Ave Occup.	Distance (ft)	Ave Occup.	Distance (ft)
1	3.1	15	0.6	15	6.5	900	2.1	900
2	3.4	15	1.5	15				
3	0.7	13	0.2	13	1.3	2100	2.4	2100
4	0.6	14	2.1	15				
5	0.0	14	0.0	15				
6	3.3	12	0.0	13	5.3	1400	0.4	1400
7	2.0	12	0.4	13				

Key:

Block #	Block Desc.
1	Wyoming to Mt.Vernon
2	Mt.Vernon to Potomac
3	Potomac to Kingsley
4	Kingsley to Sylvan

Block #	Block Desc.
5	Sylvan to Banks
6	Banks to Oak Grove Ave
7	Oak Grove Ave to Island Hill/ Pine Banks

These parking counts confirm that parking is significantly underutilized along most of the corridor. This is likely due to a combination of factors: 1) all the properties in this corridor have off-street parking for residents and customers, 2) a majority of the properties are single family residential and do not have as many visitors as other land use types.

Consolidating parking along one side of the street in locations where it is significantly underutilized frees up a lot of space to make lower Main Street safer and more enjoyable for everyone who lives and travels along it. The following design recommendations describe how the City and residents can make such improvements, block by block.

We recommend all crosswalks, both along and across Main St, be converted to higher visibility continental (“zebra”) crosswalk format. To improve pedestrian access throughout the corridor, we recommend adding five (5) new crosswalks across Main Street at Mount Vernon St, Potomac St, Lodge Ave, Woodruff Ave, and Ledgewood Ave. These crosswalks are spaced roughly equally throughout the lower Main St corridor, and are important elements to enable access to bus stops, between neighborhoods on either side of Main Street, and to provide safe and convenient access to the consolidated on-street parking on the east side of Main Street.

We recommend daylighting all crosswalks to ensure better visibility of pedestrians stepping out into the street. Daylighting also benefits motorists looking for an opening in cross traffic when pulling out of side streets onto Main St. At intersections where there are potential conflict zones between motorists entering and exiting side streets, we recommend green paint to emphasize the bike lanes. We also recommend green-painted bike boxes at signalized intersections and green-backed sharrows where the bike lanes become shared bike/bus stop zones.

Figure 2 illustrates the preferred design details for the overall recommendations described above. The following sections provide specific descriptions of the recommended improvements, including some options for alternative configurations. Figure 3 and 6 show the minimum recommended improvements of unprotected, unbuffered bike lanes. However, for very little additional effort and resources, the configuration shown in Figure 2 and 7 provide immensely greater benefits.

Design Recommendations:

Wyoming Ave to Ledgewood Ave

Maintaining the on-street parking in front of the six apartment buildings on the west side of Main Street, from 287 to 314 Main Street, is important to serve residents and visitors to those buildings. Removing the two spaces on the east side of this block allows a 7-foot wide median and 5-foot wide northbound bike lane, with the travel lanes narrowed to a safer width of 11-feet. See Figure 2 for design details.

An average of just 6 cars are parked over 1.25 miles of curb on Main Street from Potomac St to Banks Place.



South of 287 Main Street, we recommend parking be consolidated on the east side of the street, with no parking on the west side. Again, it is within this section of the roadway that the committee observed extremely low rates of on-street parking, and ample on-site parking for existing residents and business customers. The remaining roadway space can then accommodate “protected” bike lanes¹ with a striped buffer and safe hit bollards in both directions along with two 11-foot wide travel lanes and parking along the east side of

the street. The parking would sit outside of the bike lane, providing additional protection to cyclists. Because of the existing curb-to-curb dimension in this section of lower Main Street, it is impossible to stripe even minimum dimension bike lanes without removing parking on at least one side of the street. Adding substandard bike lanes will result in cyclists riding in the “door zone” where cars are parked. It will also fail to encourage less confident riders to bike. See Figures 2 and 7 for preferred design details.

Alternative Design Options

With the space provided by consolidating parking to one side of the street through this section of lower Main Street, there are other options. All of these options would provide even more protection and comfort for people riding bikes, which is important for accommodating people who may feel more cautious about riding in traffic, such as families with younger kids, women, and older riders.

One option is to upgrade the buffered bike lanes to raised raised “cycletracks” with the curb moved out to the edge of the buffer, and the surface raised up with asphalt to match the height of the sidewalk. See Figures 4 and 9 for details.

¹ Definition of a Protected Bike Lane: <https://peopleforbikes.org/placesforbikes/the-placesforbikes-style-guide/#pbldefinition>

Bike facilities could also be built as a two-way path, similar to a shared-use path like the Northern Strand Community Trail.² A two-way design be located on either the east or west side of the street, and could be painted on the roadway, or could also be built as a raised cycletrack. See Figures 5, 10, and 11 for a drawings showing these alternatives.

While the two-way design makes better use of space by sharing a buffer for both directions of bike traffic, it can be more complicated to transition through signalized intersections and between typical bike lanes on both sides of the street. Routing cyclists from existing one-way bike lanes on either side of the street to and from the two-way facility requires reconfiguration of the intersections where the path begins and ends, to ensure bicyclists can safely cross the roadway. Often this is accomplished with special bike signals that allow cyclists to ride diagonally through an intersection while all vehicle traffic stops. This can be done concurrently with pedestrian crossings to reduce travel delay.

Ledgewood Ave to Banks Place

Between Ledgewood Ave and Sylvan St, the curb-to-curb dimension on Main St widens from 42-feet to 48-feet. This increased width accommodates a left turn lane, which we recommend for southbound vehicles from Main St that want to turn onto Sylvan. This change leaves a single southbound through lane traveling on toward Banks Place. Currently, motorists frequently treat this wider segment of roadway as if the road expands to two southbound lanes. This leads to increased speeding and reduced comfort for people walking and bicycling. Midway between Sylvan St and Banks Place, Main St widens further to 50-feet. See Figure 2 for preferred design details.

The segment of Main Street between Sylvan St and Banks Place features a high number of right turns in both directions. Northbound motorists turn onto Sylvan to reach points north and east, and southbound motorists turn onto Banks Place to access the MBTA Orange Line. Motorists also make frequent turns in and out of the commercial parking lot for East Boston Savings Bank, the YMCA child care facility, and Hunt's Photo. These high turn volumes pose a hazard to bicyclists in the roadway, and pedestrians on the sidewalk.

We recommend using the excess curb-to-curb space in this block to better demarcate space for vehicles and bicyclists, so they can more safely coexist on this busy stretch of Main St. Two options are shown in this report, but other variations are possible if there is an interest in combining elements, or phasing in a new design overtime.

² Multi-use, two-way trail configuration: <https://biketothesea.org/map/>

Sylvan to Banks - Option 1

Option 1 keeps the bike lanes in a protected, buffered zone along the curb and also separate bikes and motorists at the intersections with both Sylvan Street and Banks Place. See Figure 2 for this preferred alternative. The lane is buffered where it doesn't conflict with the bus stop or curb cuts into the parking lot on the west side, and continuously buffered on the east side. These buffer areas are shown with "safe hit" posts to further separate bicyclists and motorists. The most innovate element of this design option is the "protected intersection" features that continue the separation of vehicles and bicyclists through the intersections where the right turn volumes are high.³ This design would require reconstruction of the curbs on the west side of the Banks Place intersection, and the southeast side of the Sylvan intersection, and installations of raised islands that protect the bike lane as it enters the intersection.

This design is far safer than existing conditions, and also more comfortable for riders who are less confident sharing the lane with vehicle traffic. As funding becomes available, we recommend this design be complimented with a separated signal phase to with new signal hardware to maximize safety and separation of modes. The phasing works as follows: Motorists are given a green right turn arrow, while a separate bike signal shows a red light to bicyclists and "Don't Walk" signal for pedestrians. This allows right turning vehicles to clear out of the lane without conflicts with bikes or pedestrians. Then, the right turn lane gets a yellow, then red arrow, preventing motorists from turning right. During this phase, bicyclists get a green light, and pedestrians get a "Walk" phase. Bicyclists turning right are required to yield to pedestrians as they normally would on a right turn.

The benefits of this configuration include more complete separation of bicycle and pedestrian movements from vehicle movements. In addition, this positions the right turn lane away from the curb, which should make it easier for the bus to maneuver around the corner when turning onto Banks Place. A set back stop bar is likely still necessary to ensure the bus has adequate space to make the turn, however it does not need to be as long as the one shown in Option 2. While no bus routes are routed from Main Street to Sylvan Street, Figure 2 shows an example of the swept path of a WB-40, which is large than a MBTA bus, should such routing be required in the event of construction, special events, or other unforeseen changes.

³ Protected Intersection Informational Website: <http://protectedintersection.com/>

Sylvan to Banks - Option 2

Option 2 creates separate right turn lanes for motorists turning from Main St onto Sylvan St and Banks Place. See Figure 3 for design details. A “mixing zone”⁴ mid-block allows bicyclists to continue straight for travel along Main St, where the right turn lanes open up for motorists. Advanced signage on a raised island would alert motorists to the need to merge to the right to make right turns, which is a natural inclination. The excess roadway width provides space for 2- to 3-foot wide buffer next to the bike lanes in this block, offering bicyclists further protection. To facilitate improved bus movement in the right turn from Main St to Banks Place, a setback stop bar and “Bus Keep Clear” stencil is recommended on Banks Place. An illustration of the swept path of the bus is shown to depict how it would make this turn from the curb-side right turn lane onto Banks Place.

Banks Place to Malden Border

From Banks Place to Oak Grove Ave (the main entry drive to Oak Grove Village), the curb to curb dimension returns to 40-feet. This includes an 8-foot wide parking lane on the west side of the street. No parking is allowed on the east side

⁴ Mixing zone:
<https://nacto.org/publication/urban-bikeway-design-guide/intersection-treatments/combined-bike-laneturn-lane/>

of the street. There is ample room within the roadway to accommodate two 11-foot travel lanes and two 5-foot wide bike lanes, while maintaining the existing parking. See Figure 2 for design details.

By improving the ability of cyclists to continue along on Main Street south of Banks Place, this design recommendation will allow people cycling to Oak Grove MBTA station to continue to Oak Grove Ave and use the existing ped/bike bridge over Spot Pond Brook for access to the station. This helps cyclists avoid riding on Banks Place, which is not wide enough to accommodate bike lanes and only has one narrow sidewalk on the west side of the street, within Melrose.

South of Oak Grove Ave, the curb-to-curb dimension of Main Street widens to 48-feet with parking on the west side, no parking on the east side, and a northbound left turn lane. This turn lane is not necessary for the low volume of left turns at this location, and City of Melrose staff noted that it was likely a convenience added when the development was built, simply because there was extra right-of-way available. We recommend removing this lane and installing buffered bike lanes on both sides of the street. The existing road can accommodate 6-foot bike lanes with a 3-foot buffer, 11-foot travel lanes, and the 8-foot parking lane. The City of Malden has confirmed they can adjust the alignment of their double yellow centerline to match the new centerline that results from our recommended reconfiguration, to ensure continuity.

Main Street at Banks Place, looking north



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Preferred alternative: Protected, buffered bike lanes, pedestrian safety improvements, and traffic calming measures



Existing conditions: Lower Main Street @ Kingsley



Figure 1: Proposed bike route improvements, showing connections from "low stress" and "commuter route" to Wyoming Ave. and to Oak Grove / MBTA and City of Malden.

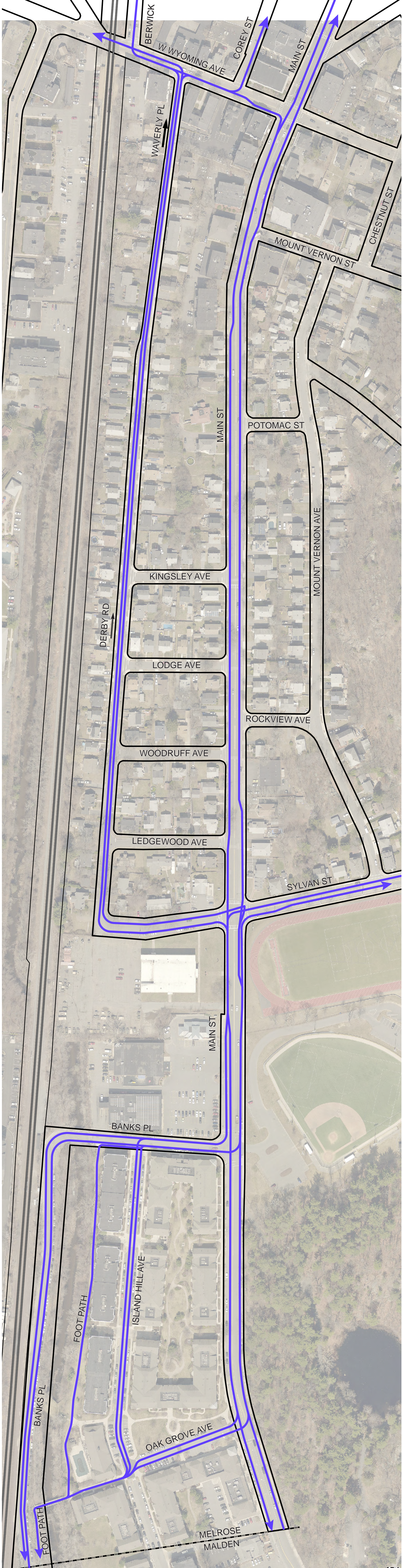


Figure 3. Minimum standard bike lanes, improved and additional crosswalks and pedestrian safety upgrades, bike lanes, consolidated parking along the east side of Main St, mixing zones at Sylvan/Main and Banks/Main.



Figure 4: Alternative design concept showing raised, protected bike lanes. Asphalt surface with a curb along roadway, surface height matches sidewalk height, ramps lead up and down to roadway elevation for intersections.

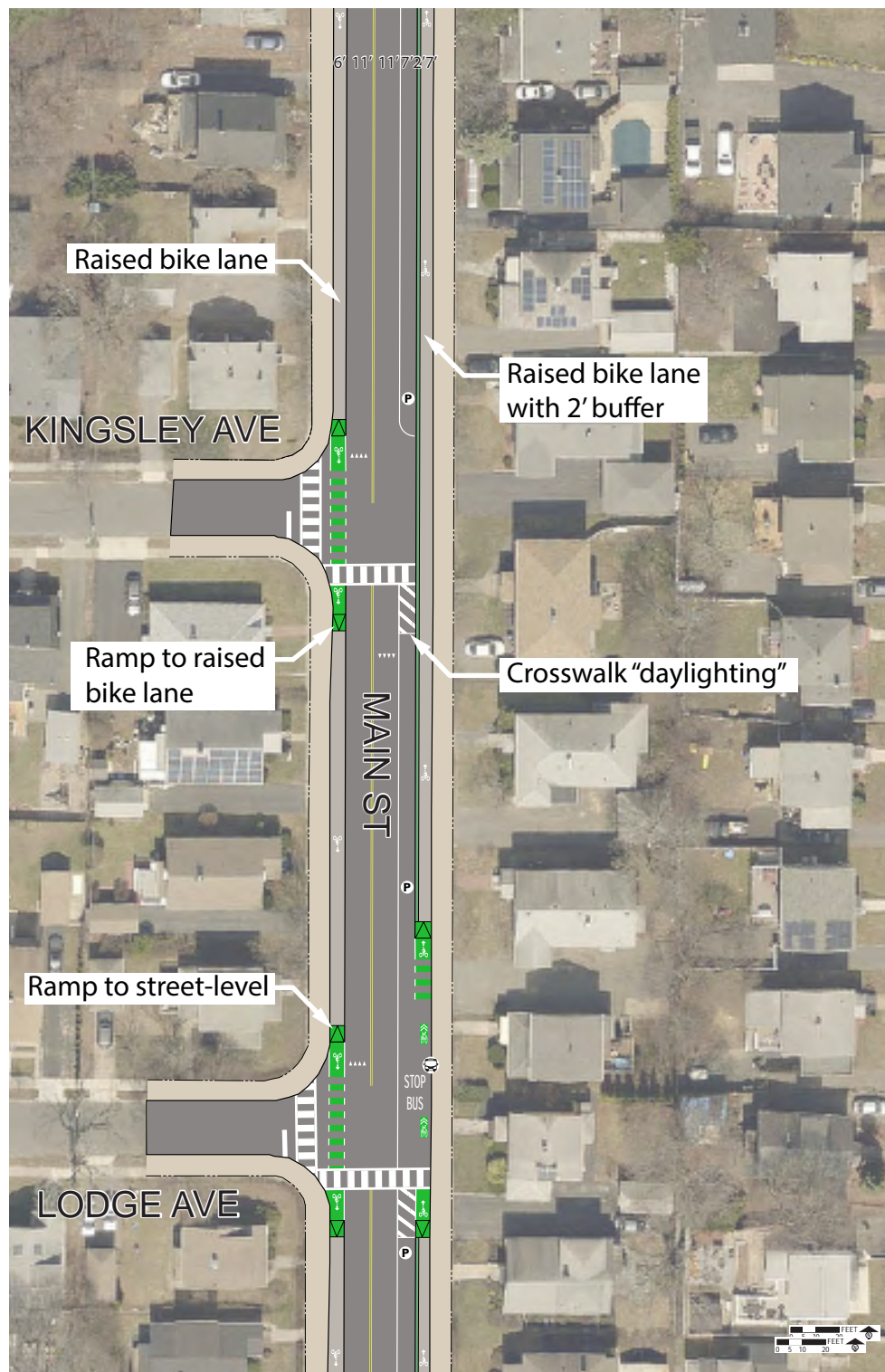


Figure 5: Alternative design concept showing two-way protected, buffered bike lane. Shown on west side but could be on the east side instead. Requires additional design at signalized intersections and when transitioning from typical bicycle lanes.

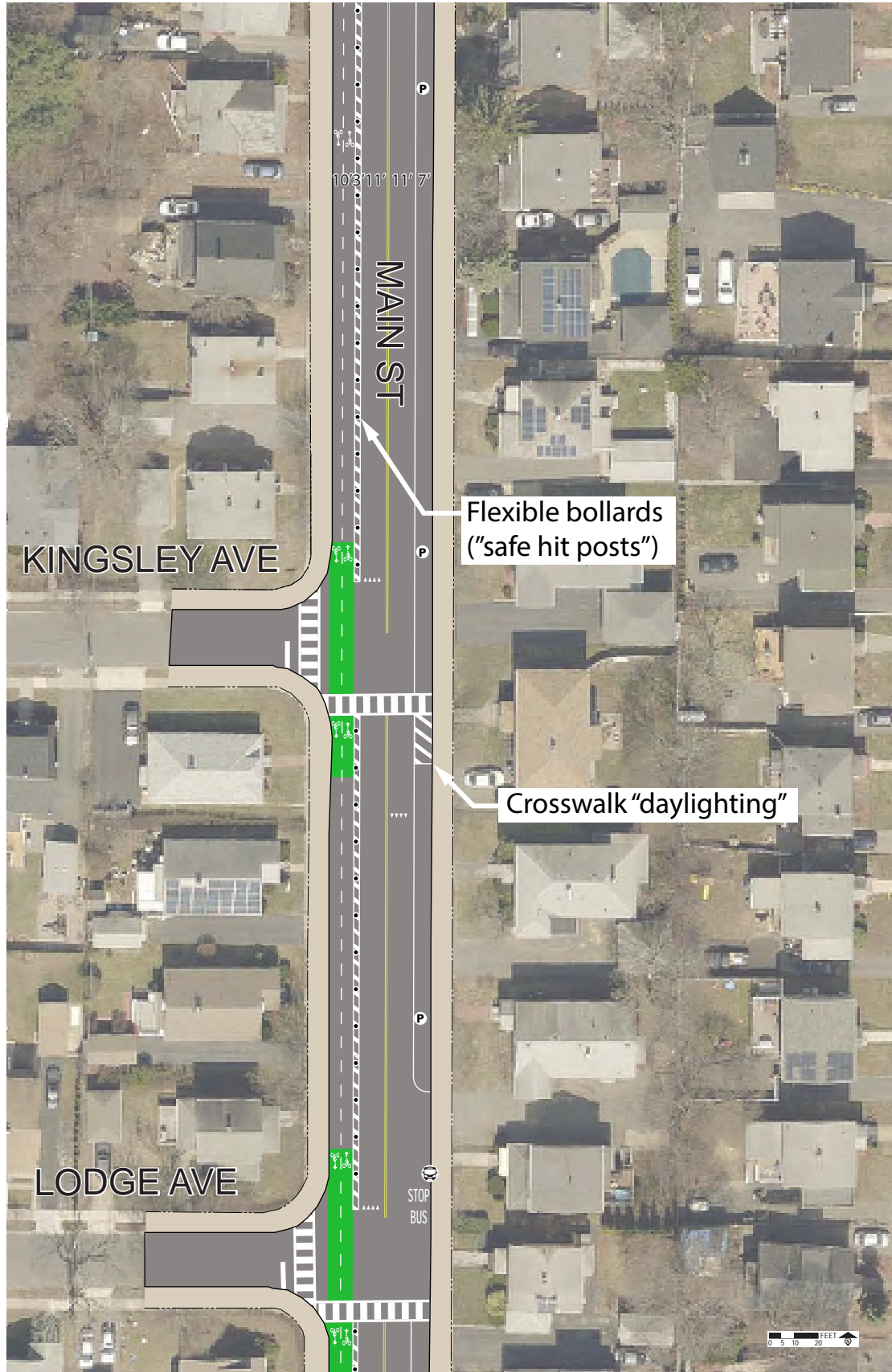


Figure 6: Lower Main Street Typical Existing Cross Section

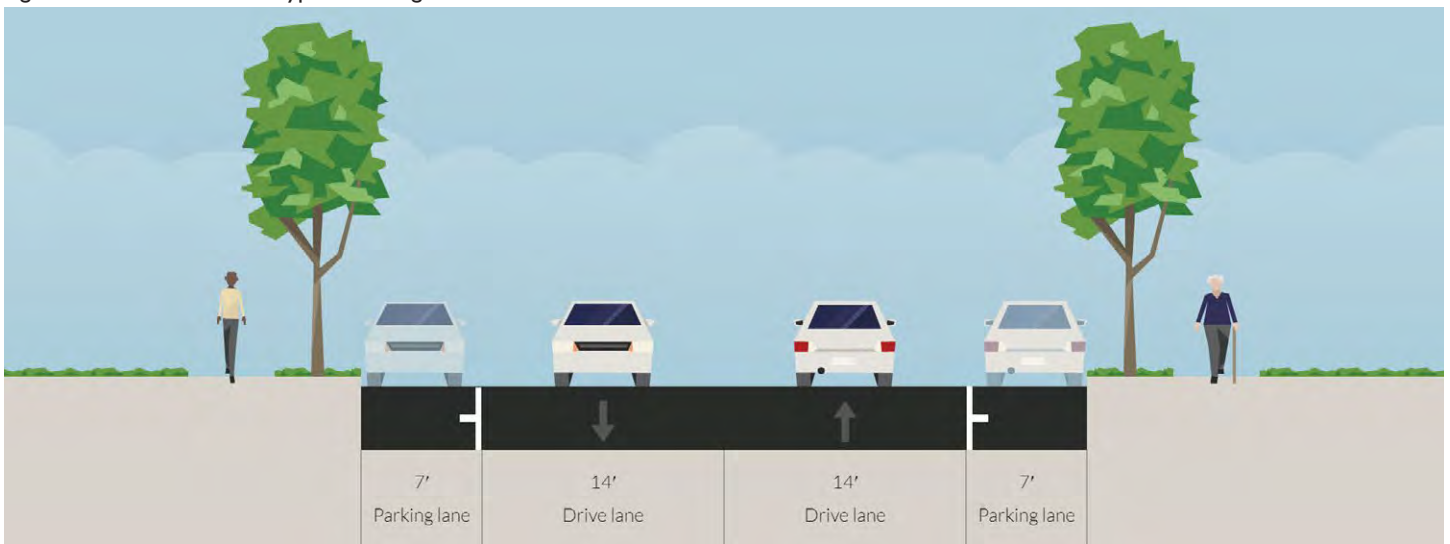


Figure 7: Lower Main Street Recommendation - Preferred Option showing buffered, protected bike lanes

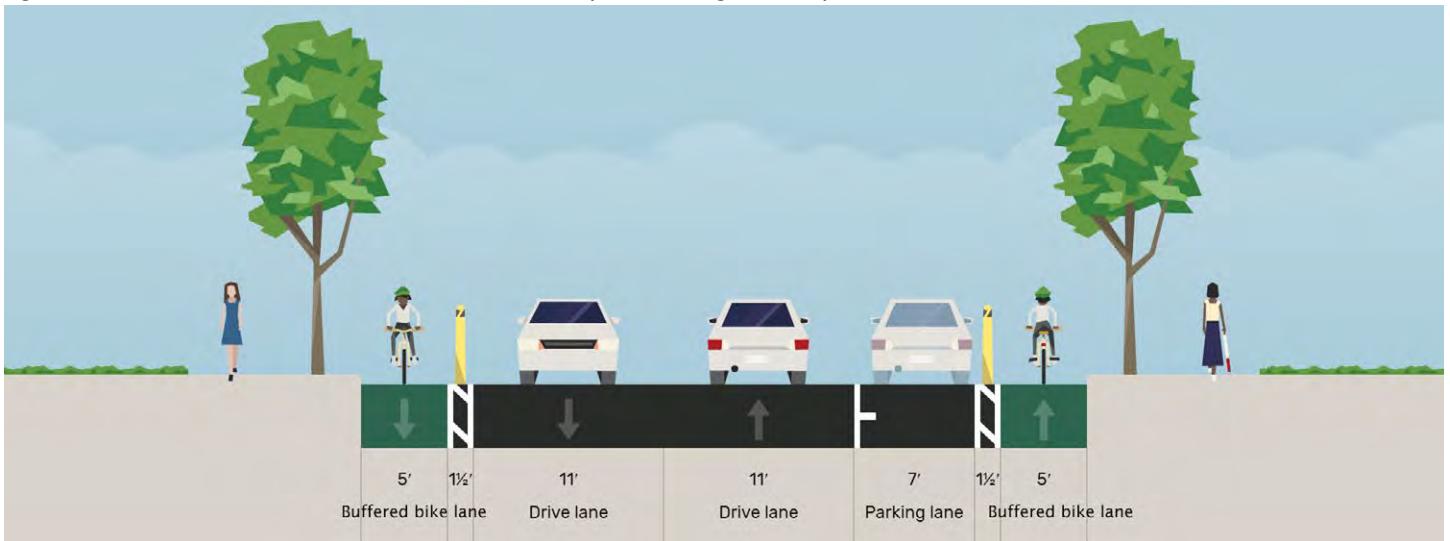


Figure 8: Lower Main Street Recommendation - Minimum conditions option showing painted bike lanes

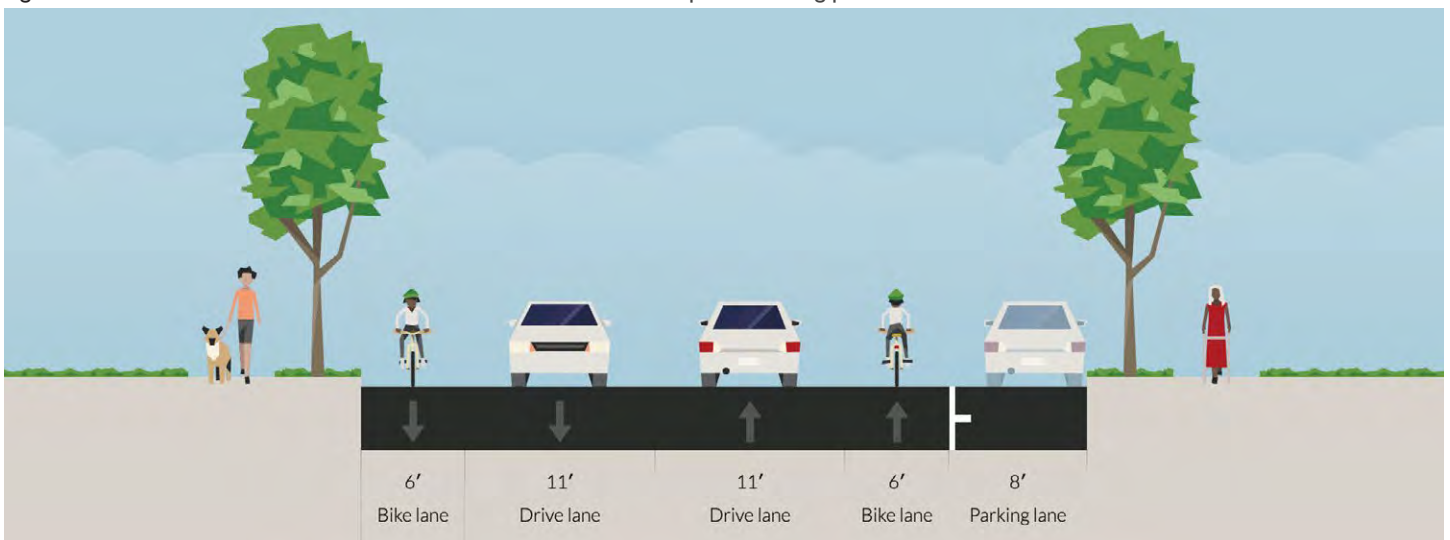


Figure 9: Lower Main Street Recommendation - Option Showing Raised Protected Bike Lanes

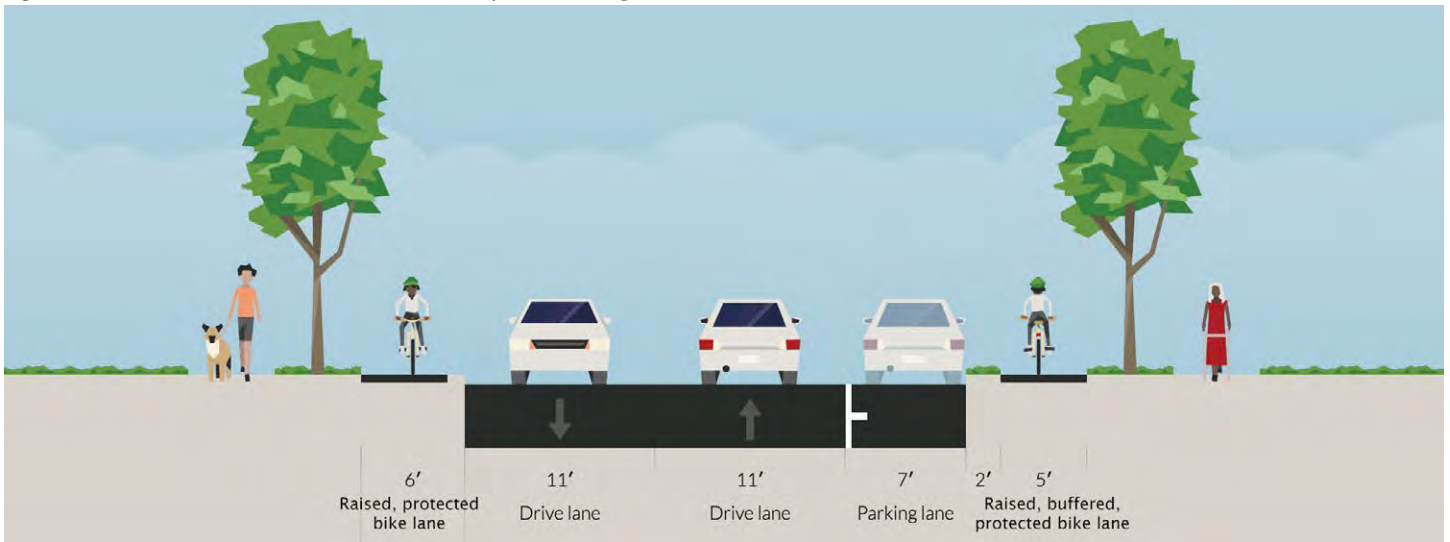


Figure 10: Lower Main Street Recommendation - Option Showing Two Way Painted and Buffered Cycletrack

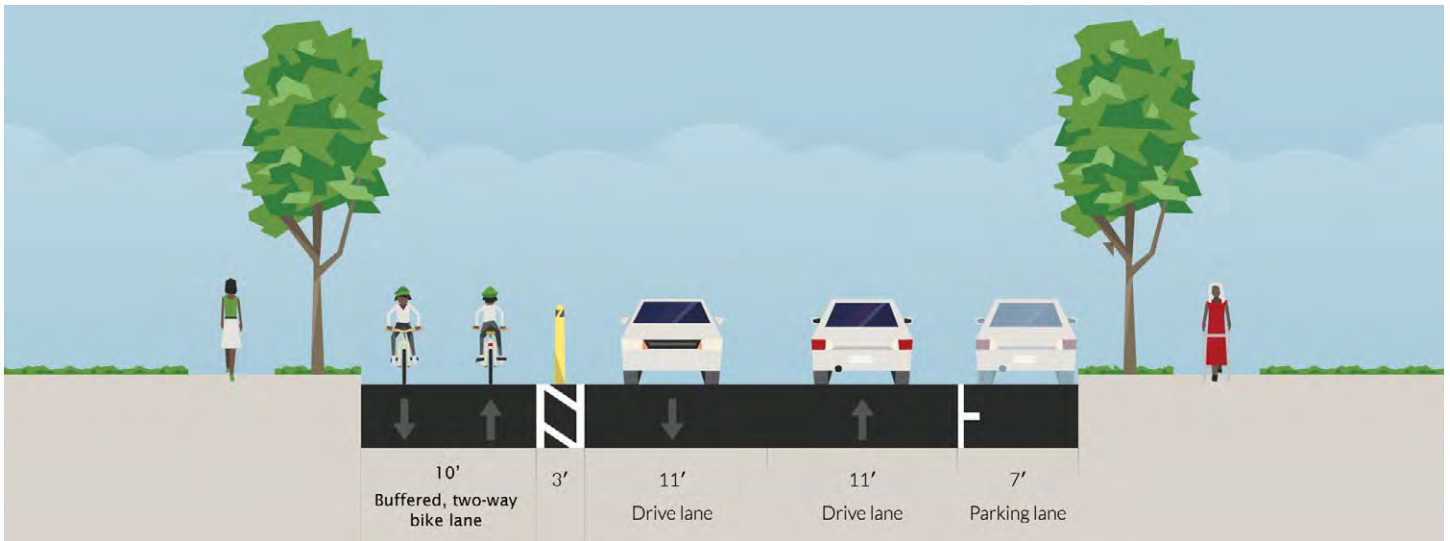
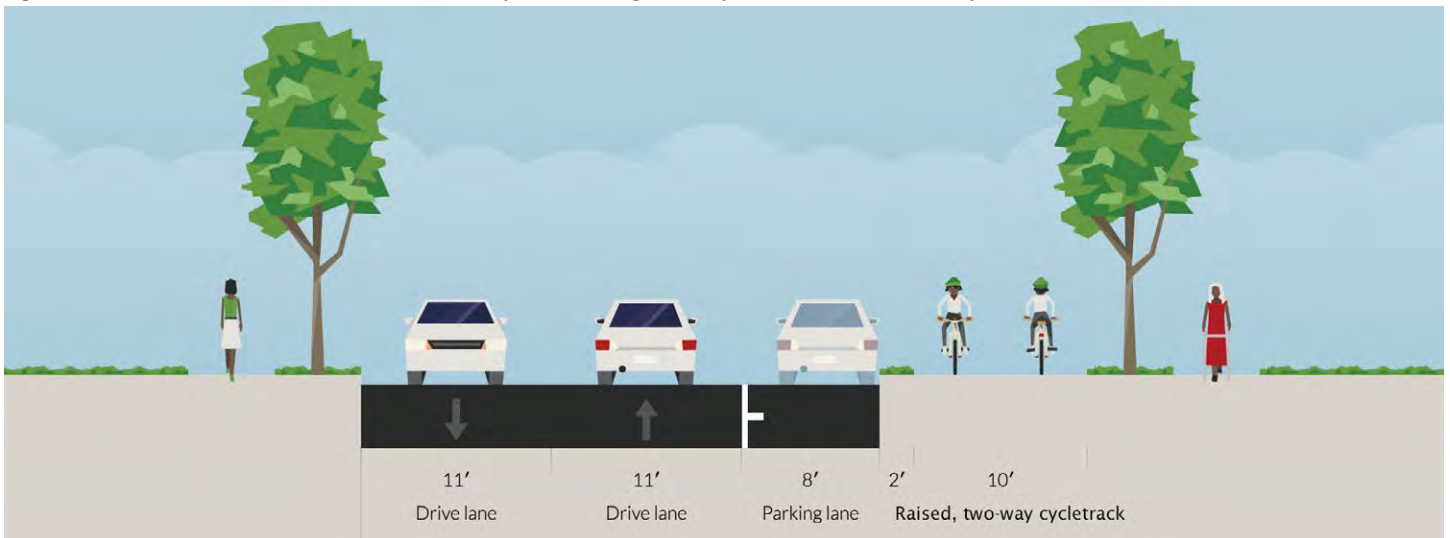


Figure 11: Lower Main Street Recommendation - Option Showing Two Way Raised and Protected Cycletrack



Appendix A: Lower Main Street Parking Observations

The parking on lower Main Street is significantly underutilized, with the exception of a few key blocks. To understand exactly where parking is used, and where it is not, the Committee recorded the number of cars parked between W. Wyoming Avenue and Island Hill Avenue (just beyond the border of Malden) over the course of a week in 2019, from September 6th to September 14th. This week was selected because it was after the Labor Day holiday, and included

two Saturdays and all the weekdays in between. Counts were taken at a variety of times, including morning, afternoon, and evening. The following tables list the number of cars parked on each block during the observed date and time. Reference Figure 1 for a map of the parking locations. Photographs are provided for several of the locations, taken during observations date/time as noted.

Recorder:	FINN MCSWEENEY	Date:	9/6/2019	
		Time:	5:45:00 PM	
Block #	Block Desc.	SB (west side)	NB (east side)	
1	Wyoming to Mt. Vernon	4	0	
2	Mt. Vernon to Potomac	4	2	
	Potomac to Kingsley	0	1	
3	Kingsley to Sylvan	1	2	
4	Sylvan to Banks	0	0	
5	Banks to Oak Grove Ave	9	0	
6	Oak Grove Ave to Island Hill/Pine Banks	0	0	

Recorder:	FINN MCSWEENEY	Date:	9/7/2019	
		Time:	8:00:00 AM	
Block #	Block Desc.	SB (west side)	NB (east side)	
1	Wyoming to Mt. Vernon	4	0	
2	Mt. Vernon to Potomac	3	2	
	Potomac to Kingsley	0	0	
3	Kingsley to Sylvan	0	0	
4	Sylvan to Banks	0	0	
5	Banks to Oak Grove Ave	0	0	
6	Oak Grove Ave to Island Hill/Pine Banks	1	0	

Recorder: FINN MCSWEENEY		Date: 9/7/2019	
		Time:	4:15:00 PM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	4	1
2	Mt. Vernon to Potomac	9	1
	Potomac to Kingsley	0	1
3	Kingsley to Sylvan	1	4
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	3	0
6	Oak Grove Ave to Island Hill/Pine Banks	4	0

Recorder: FINN MCSWEENEY		Date: 9/7/2019	
		Time:	8:15:00 PM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	6	1
2	Mt. Vernon to Potomac	5	1
	Potomac to Kingsley	1	0
3	Kingsley to Sylvan	1	0
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	3	0
6	Oak Grove Ave to Island Hill/Pine Banks	3	0

Recorder: FINN MCSWEENEY		Date: 9/9/2019	
		Time:	6:15:00 AM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	1	1
2	Mt. Vernon to Potomac	2	1
	Potomac to Kingsley	0	0
3	Kingsley to Sylvan	1	0
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	1	0
6	Oak Grove Ave to Island Hill/Pine Banks	0	0

Recorder: RYAN WILLIAMS		Date: 9/9/2019	
		Time:	8:30:00 AM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	1	0
2	Mt. Vernon to Potomac	2	0
	Potomac to Kingsley		
3	Kingsley to Sylvan	1	2
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	unk	unk
6	Oak Grove Ave to Island Hill/Pine Banks	unk	unk

Recorder: RYAN WILLIAMS		Date: 9.9	
		Time:	5:30
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	10	2
2	Mt. Vernon to Potomac	1	1
3	Kingsley to Sylvan		1
4	Sylvan to Banks		
5	Banks to Oak Grove Ave		
6	Oak Grove Ave to Island Hill/Pine Banks		

Recorder: FINN MCSWEENEY		Date: 9/10/2019	
		Time:	7:00:00 AM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	1	1
2	Mt. Vernon to Potomac	2	0
	Potomac to Kingsley	0	0
3	Kingsley to Sylvan	0	4
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	0	0
6	Oak Grove Ave to Island Hill/Pine Banks	2	0

Heading South on Main Street 9/10/2019 7:00 AM



Recorder:	Ryan Williams	Date:	9/10/2019	
		Time:	9:10	
Block #	Block Desc.	SB (west side)	NB (east side)	
1	Wyoming to Mt. Vernon	4	0	
2	Mt. Vernon to Potomac	2	0	1 vehicle left SB as I was recording.
	Potomac to Kingsley	2	0	
3	Kingsley to Sylvan	0	4	all parked between wodruff/sylvan
4	Sylvan to Banks	0	0	
5	Banks to Oak Grove Ave	2	0	1 cyclist sb, 1 pedestrian nb walking in empty parking lane
6	Oak Grove Ave to Island Hill/Pine Banks	0	4	all in bumped out parking lane

Heading South on Main Street 9/10/2019 9:10 AM



Heading South on Main Street 9/10/2019 9:10 AM (continued)



Recorder:	Ryan Williams	Date:	9/10/2019	
		Time:	5:28:00 PM	
Block #	Block Desc.	SB (west side)	NB (east side)	
1	Wyoming to Mt. Vernon	4	0	
2	Mt. Vernon to Potomac	5	3	
	Potomac to Kingsley	2	1	
3	Kingsley to Sylvan	1	2	7 bikes nb, 1 bike sb
4	Sylvan to Banks		0	
5	Banks to Oak Grove Ave		0	
6	Oak Grove Ave to Island Hill/Pine Banks		0	

Heading North on Main Street 9/10/2019 5:28 PM



Heading North on Main Street 9/10/2019 5:28 PM (continued)



Recorder:	FINN MCSWEENEY	Date:	9/11/2019	
	https://photos.app.goo.gl/QCSbnbJ4T3NmHFJq8	Time:	6:15:00 AM	
Block #	Block Desc.	SB (west side)	NB (east side)	
1	Wyoming to Mt. Vernon	1	1	
2	Mt. Vernon to Potomac	2	1	
	Potomac to Kingsley	0	0	
3	Kingsley to Sylvan	0	3	
4	Sylvan to Banks	0	0	
5	Banks to Oak Grove Ave	0	0	
6	Oak Grove Ave to Island Hill/Pine Banks	0	1	

Heading South on Main Street 9/11/2019 6:15 AM



Recorder: Ryan Williams		Date: 9/11/2019	
		Time:	9:40:00 AM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	2	2
2	Mt. Vernon to Potomac	2	2
3	Kingsley to Sylvan		4
			All in front of Melrose Glass
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave		
			1 cyclists
6	Oak Grove Ave to Island Hill/Pine Banks		

Recorder: FINN MCSWEENEY		Date: 9/12/2019	
		Time:	6:10:00 AM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	1	1
2	Mt. Vernon to Potomac	1	1
	Potomac to Kingsley	0	0
3	Kingsley to Sylvan	0	0
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	0	0
6	Oak Grove Ave to Island Hill/Pine Banks	0	0

Recorder: FINN MCSWEENEY		Date: 9/13/2019	
		Time:	7:00:00 AM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	1	0
2	Mt. Vernon to Potomac	3	3
	Potomac to Kingsley	0	0
3	Kingsley to Sylvan	0	3
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	2	0
6	Oak Grove Ave to Island Hill/Pine Banks	2	0

Recorder: FINN MCSWEENEY		Date: 9/14/2019	
		Time:	1:00:00 PM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	6	0
2	Mt. Vernon to Potomac	3	2
	Potomac to Kingsley	2	0
	Potomac to Kingsley	2	0
3	Kingsley to Sylvan	0	2
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	9	0
6	Oak Grove Ave to Island Hill/Pine Banks	5	0

Recorder: FINN MCSWEENEY		Date: 9/14/2019	
		Time:	6:15:00 PM
Block #	Block Desc.	SB (west side)	NB (east side)
1	Wyoming to Mt. Vernon	7	1
2	Mt. Vernon to Potomac	6	4
	Potomac to Kingsley	2	0
3	Kingsley to Sylvan	3	2
4	Sylvan to Banks	0	0
5	Banks to Oak Grove Ave	11	0
6	Oak Grove Ave to Island Hill/Pine Banks	7	0

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